

A dirt path winds through a dense forest of tall, thin trees with vibrant green foliage. Sunlight filters through the canopy, creating dappled shadows on the path. Large, white, stylized numbers '05' are overlaid on the right side of the image, with a thin green outline around them. The path leads into the distance, flanked by various green plants and shrubs.

05

IMPLEMENTATION



Implementation

City leaders should utilize this document to prioritize, plan, and budget the implementation future trail projects. McKinney's implementation strategy should be prioritized into immediate projects, short-term priorities, and future goals. This section also summarizes probable costs of trail construction and identifies potential funding sources to supplement city capital dollars.

IMMEDIATE PRIORITIES (1 TO 2 YEARS)

Projects that provide strategic connections and greatly increase the impact the trail network for McKinney citizens and visitors should be identified as immediate priorities. Typically a high-priority for current trail users these segments make critical links and address a direct mobility or safety need.

SHORT-TERM PRIORITIES (3 TO 9 YEARS)

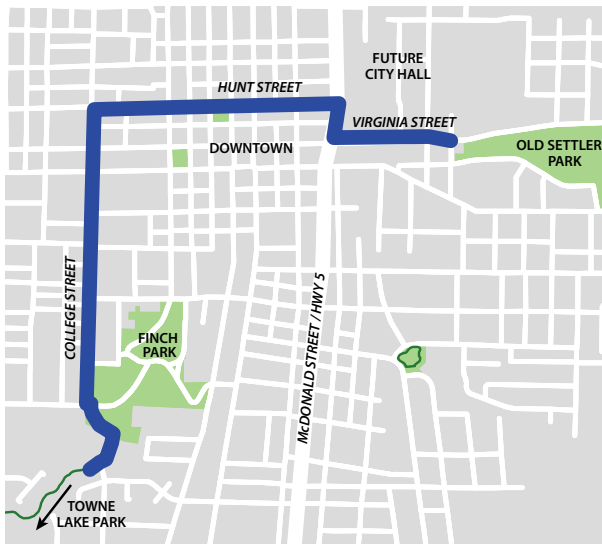
Short term projects are those which there is a current need but require additional time for design, coordination and execution. Examples of these project types include signature trail extensions, park loops associated with new park development, and critical links between established districts.

FUTURE PRIORITIES (10+ YEARS)

Trail segment priorities beyond those listed as initial priorities may be planned as longer-term goals (10 or more years) for trail network development in McKinney. The order of these future projects will depend on an assortment of variables related to overall development in the city including phasing and implementation of the thoroughfare plan, development patterns / community growth, land acquisitions and the availability of unforeseen funding sources such as grants, partnerships, donations, etc.

CATALYST PROJECTS

This plan recommends an extensive network of trails that will be constructed over time. These proposed trails will be implemented through collaboration and commitment by the City and development partners. The following pages identify six catalyst projects that will make immediate critical connections. Projects were determined using stakeholder input, staff recommendations and site observations. The proposed trail segments will provide improved network connectivity, comfort and safety for trail users along these key connections.



Downtown Connection

Extending from Old Settlers Park to Towne Lake Park, this project will connect the Downtown Districts neighborhoods and destinations to trails in the Wilson Street greenbelt. From Old Settlers Park, 1.5 miles of roadside trails will follow Virginia Parkway, cross McDonald then head east along Hunt, turn south next to College Street then connect to Finch Park. An additional ¼ mile trail will follow the creek linking Finch Park and existing trails that extend to Towne Lake Park. Trails and extend along the creek to existing trails at Towne Lake Park.



McKinney Greens

This former golf course property around the McKinney Green's neighborhood currently has undulating asphalt cart paths which function as nature trails. This project proposes to reconstruct these pathways to meet current trail design standards into a 2 mile trail loop that then connects the Towne Lake Park trail system via an underbridge crossing of Eldorado Pkwy.



Rowlett Creek Extension Craig Ranch

Rowlett Creek Park includes a ¾ mile trail that follows the creek. This project includes new trails in both directions. The North trail extension is ¾ mile, crossing under Silverado, the alignment then crosses the creek via pedestrian bridge then extends along the west side of the creek until Custer Road. The south extension includes a short 350 lf link between the Stacy Road under-bridge crossing and the existing trails along Settlement Way. This project creates continuous trail from US 380 to McKinney's southern City limit.

CATALYST PROJECTS



Erwin Park Connection

This project includes 1.5 miles of trail connecting trails and recreation opportunities at Erwin Park, the Ottawa Parcel, and Park Property to planned trail segments and new residential neighborhoods. The trail will follow the new alignment of Bloomdale Rd with one crossing at CR 1006, then extend in through the park property where it will connect to the Painted Tree spine trail.



Cottonwood Creek Connection

The trail along Collin McKinney Parkway provides an east-west travel in south McKinney, connecting neighborhoods to Emerson High School and the Craig Ranch Sport Complex. The trail currently ends at the Lake Forest intersection. This project will extend the trail east to Cottonwood Creek where the alignment will run south, along the east side of the creek and under the SH 121 bridge to the city limits. The $\frac{3}{4}$ mile segment will connect to Allen's existing 3-mile Cottonwood Creek Trail creating a significant regional connection for McKinney.



Auburn Hills - Gray Branch Connection

Heavy vehicular traffic on US 380 is an imposing barrier for many bicyclists and pedestrian, effectively separating north and south Mckinney. This project includes a .23 mile trail and grade separated crossing beneath US 380, linking the Auburn Hills neighborhood to the Gray's Branch Park property eventually connecting to the trails in the Wilson Creek Greenbelt.

COST OF IMPLEMENTATION

Trail projects include similar elements such as pavement, signage and markings, moderate grading that can be used estimate cost for a basic 12' trail at \$800,000 to \$1M per mile. Often trail projects include roadway intersections improvements and crossings, or recreational elements such as trailheads, rest areas and exercise stations. Trails commonly traverse undulating topography, with drainage issues, with dense vegetation or other obstacles that and may require features such as bridges, boardwalks, walls, or structures. Complete trail projects incorporating these elements can exceed \$1.5 million per mile.

On-Street bicycle facilities can be implemented on existing streets with minimal roadway modifications improving connectivity where trails are not feasible. New bike lane markings and signage improvements range in cost between \$75,000 and \$100,000 per mile.

Shared Lane markings and signage are approximately \$25,000 per mile. When budgeting for individual projects, it is recommended that certain soft costs be included in addition to estimated construction costs. They may include, but are not limited to:

- a) *Construction Contingency* - Provides a budgetary allowance for incidental or unexpected costs that can arise during the course of construction. A typical allowance for contingency is in the range of 10% to 20% of construction cost.
- b) *Owner's Costs* - These costs describe items incidental to construction that are typically the responsibility of the Owner, and can include but are not limited to land costs, geotechnical engineering, accessibility reviews, permitting, inspections, and others. A typical project allowance for Owner's Costs falls in the range of 1.5% to 2% of the total construction cost.
- c) *Market Escalation Allowance* - This line item enables the Owner to prepare for potential inflation when developing an overall budget for a project that may not be constructed for a number of years. In recent years, the COVID pandemic has resulted in global supply chain and labor disruptions leading to increased volatility in construction pricing. Current inflation is trending upward and costs are continuing to rise. A typical placeholder of 3% to 5% of construction costs is widely accepted as a reasonable allowance. However, this allowance fluctuates directly with current market conditions and is difficult to predict.



Trail Under Construction

POTENTIAL FUNDING SOURCES

Once a budget number is reached, it is important for City leaders to consider what strategic funding sources might be available to help the city supplement the cost of trail network expansion. Some of the following sources could be part of the overall implementation plan.

Collin County

- Open Space / Trail / Preserve Programs

North Central Texas Council of Governments (NCTCOG)

- Transportation Alternatives Set-Aside Program
- Regional Toll Revenue Program

State of Texas

- Texas Parks & Wildlife
 - *Outdoor Recreation Grants*
 - *Recreational Trails Grants*
- Texas Department of Transportation (TxDOT)
 - *Safe Routes to School Program*
 - *Transportation Alternatives Program*

Federal Transportation Funding Sources

- Highway Bridge Replacement and Rehabilitation Program (via TxDOT)
- Highway Safety Improvement Program (via TxDOT)
- National Highway System (via TxDOT)
- Surface Transportation Program (via TxDOT)
- Federal Transit Administration Programs
- Interstate Maintenance (through TxDOT)

Other Federally Funded Programs including Bicycle and Pedestrian Opportunities

- FHWA Pedestrian and Bicycle Funding Opportunities
- U.S. Department of Housing & Urban Development (CDBG)
- U.S. Army Corps of Engineers (USACE)
- U.S. Department of Interior (USDOI)
- National Park Service's Rivers, Trails & Conservation Assistance
- National Fish & Wildlife Resilient Communities

Miscellaneous Funding Sources

- Land Trusts
- Communities Foundation of Texas
- Meadows Foundation
- Corporate Grants, Partnerships & Sponsorships
- Foundation Directory Online
- Private Donations
- Partnerships with Volunteer Groups